

Today's Agenda

- Funding cycle overview
- TIB expectations
- Small City Arterial Program (SCAP)
- Small City Sidewalk Program (SCSP)
- Small City Preservation Program (SCPP)
- Other funding opportunities
- TIB program feedback

Small City Desired Outcomes



Street Outcomes

TIB funds projects that:

- Construct streets with good surface and stable base
- Are built on top of utilities in good condition
- Reverse the negative economic signal of failed infrastructure

System Outcomes

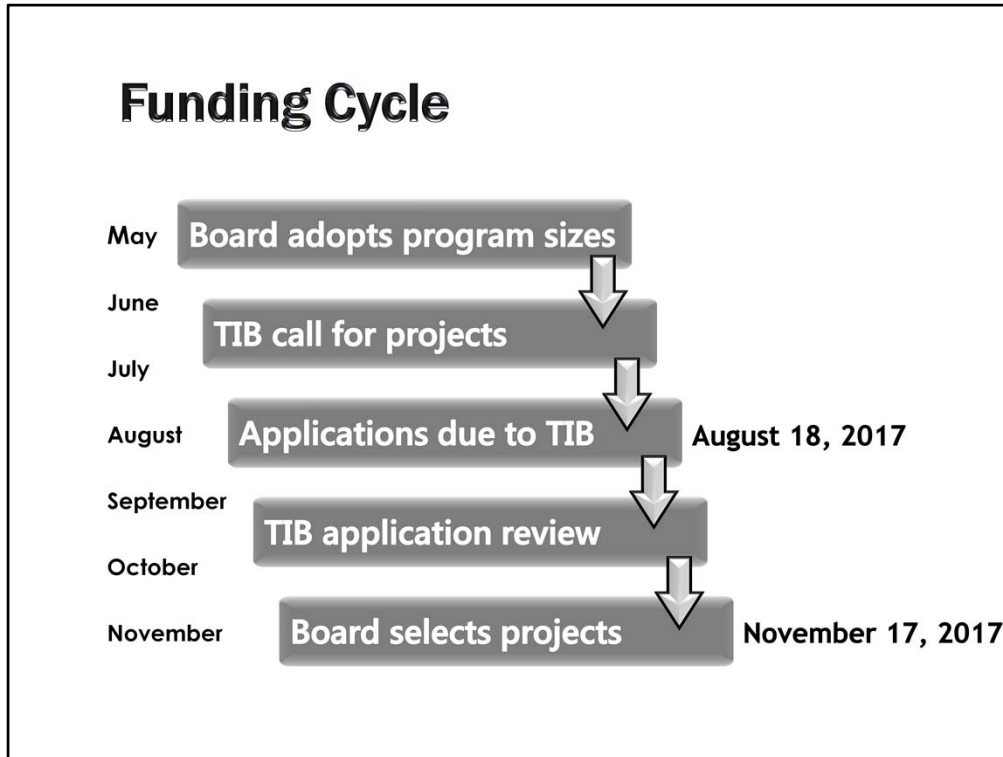
TIB funds projects that:

- Build a sidewalk network between significant generators
- Establish continuity of street system improvements

Value Outcomes

TIB funds projects that:

- Leverage the best overall value
- Eliminate wastes by achieving economies of scale and capitalize on optimal price opportunities
- Build appropriate features and structure for various conditions and need



Where to find TIB applications

- Download from TIB website at www.tib.wa.gov

How to submit your TIB applications

- Submit one originally signed application and attachments to TIB at Post Office Box 40901, Olympia, WA 98504-0901

AND

- **Email your excel application workbook to the TIB Engineer shown on the application**
- Email proposed roadway section

Application deadline

- Applications must be postmarked no later than **August 18, 2017** to be considered

Application scoring

- Applications are entered and scored by TIB engineers
- TIB engineers review application information in field
- Application ratings are reviewed for accuracy and consistency

Staff evaluation

- All applications evaluated based on their rating, scope, cost and schedule

Board selection

- Staff presents recommended projects to Board
- The Board selects projects for funding at the November board meeting

TIB Evaluation Process

- Initial review
- Field review
- Consistency review
- Jury process
- Final recommendation

Initial review

- Enter project information into rating system
- Ensure all application information is provided

Field review

- Review existing conditions
 - Verify information from application
 - Evaluate proposed improvements

Consistency review

- Ensure ratings are uniform and accurate

Jury process

- Staff discussion of project
 - How well does project address deficiencies
 - Review scope, schedule and budget
- Consider agency performance, inventory and ability

Final recommendation

- Staff recommendation presented to Board

TIB Application Expectations

- Agency priority
- Project schedule
- Project cost estimate



For City Hall,
We loved the new
sidewalk that you guys
did because it's safe
and it looks cool you
guys took all that time
just to keep us safe
Thank you very
much! You guys
Rock!

By, Kathy. Thank you

Agency priority

- Apply for projects with high importance to your agency

Project schedule

- Typical schedule for small city projects
 - First year – complete design work and acquire right of way if needed
 - Second year – construct project
 - Preservation or simple scope projects may be designed and constructed in the first year

Project cost estimate

- Ensure cost estimate includes all components of work
- Contingency reflects project complexity and uncertainty
- Estimate reviewed and signed by a professional engineer registered in the state of Washington

TIB Project Expectations

- Grant agreement execution
- Engineering oversight
- Project start
- Project funding
- Project issues
- TIB payment requests

Grant agreement execution

- RCW 47.26.084 specifies an agency must provide written certification of full project funding by returning the signed grant agreements to TIB within one year of selection
- Staff expects execution within three months of selection
 - Must have full project funding
 - SCAP and SCSP project must be included on your agency-adopted Six Year Transportation Improvement Program (TIP)
 - TIB cannot execute grant agreement until adopted TIP showing project is submitted
 - You are **not required** to include SCPP projects on your TIP

Engineering oversight

- WAC 479-05-030 specifies a professional engineer registered in the state of Washington is required on all projects using TIB funding
- Engineering is limited to 30 percent of the eligible contract cost plus construction other
- Engineering for less complex projects should be less than the engineering max

Project start

- Do not start project work until you receive approval from TIB

Project funding

- Contact your TIB engineer if any change in funding partners or commitment

Project issues

- Contact your TIB engineer to discuss issues that affect scope, schedule or budget
 - Do not amend project scope, schedule or limits without approval from TIB

TIB payment requests

- You may request payment from TIB after your grant agreement is executed
- During design, request payments at least quarterly
- Request monthly payments during construction
- Five percent of TIB funding is held until contract completion paperwork is processed

Small City Arterial Program (SCAP)

- Projects restore and improve the arterial roadway system consistent with local needs
 - Address safety
 - Improve arterial street conditions
 - Connect public facilities

SCAP Regional Distribution

\$15 Million Statewide



Region	Funds	Percent
East	\$8.2M	54.5%
Puget Sound	\$2.6M	17.6%
West	\$4.2M	27.9%

SCAP Funding is distributed regionally

- Three regions are used: East, Puget Sound and West
- Percentages are based on small city populations updated annually by the Office of Financial Management

SCAP Application Requirements

- Eligibility



Eligibility

- Incorporated cities and towns with populations less than 5,000 are eligible
- Eligible streets
 - Street must be:
 - City owned
 - Paved and connected to other paved streets
 - Does not include alley ways
 - Does not include dead end streets unless they provide access to significant traffic generators
 - New streets and gravel streets may be classified by the Board when a paved street would qualify as an arterial
 - TIB street system maps indicating pavement condition are available for your agency at the following link:
 - <http://www.tib.wa.gov/TIBDashboard/index.cfm?PCR>
 - From the state map click on your county, click on your city
 - Select 'Street System' from the lower right hand corner of the screen

SCAP Application Requirements

- Local match
- Project cost estimate
- WSDOT concurrence

Noneligible costs **are not** considered part of your local match

Local match can come from any source other than TIB

- Local match includes contributions from your city, other agencies, federal and/or private sources
- The local match requirement for your agency is shown when you select your agency name from the dropdown on the application form

Minimum local match

- Based on agency assessed valuation updated annually by the Department of Revenue
- | | |
|----------------------------------|---------------|
| - Assessed Valuation | Minimum Match |
| - Under \$100 million | 0 percent |
| - \$100 million to \$500 million | 5 percent |
| - Over \$500 million | 10 percent |

Project cost estimate

- Ensure cost estimate includes all components of work
- Contingency should reflect project complexity and uncertainty
- Estimate must be reviewed and signed by a professional engineer registered in the state of Washington

WSDOT concurrence

- Written WSDOT concurrence required with application for projects on or connecting to a state highway right of way

SCAP Project Attributes

- Project types
- Design requirements



Project types

- Full reconstruction - rebuild roadway base and surfacing
- Full depth reclamation – recycle the existing roadway to strengthen base with new surfacing
- Overlay – provide pavement repair and add surfacing
- New street - construct new connection in agency’s street system
- Project may be a combination of types

Design requirements

- Proposed section must meet AASHTO standards at a minimum
- Sidewalk is required only for reconstruction or full depth reclamation when:
 - Project is located in the business district
 - Project connects pedestrian destination with business district
- TIB sidewalk standards
 - Sidewalk must be ADA compliant
 - Minimum width five feet clear
 - Hard, smooth surface (concrete, asphalt)
 - Separated from travel lanes with curb or physical barrier (e.g. drainage swale)
- TIB considers sidewalk deviations at application
 - Include your deviation request with the application
 - Deviations are granted for locations where sidewalk is not needed or unsafe

SCAP Project Attributes

- Project limits
- Typical grant amount

Project limits

- Project should have logical limits
 - Do not leave a short street section unimproved

Typical Grant Amount

- Typical funding ranges from \$300,000 to \$750,000 per project
- Contact your TIB Engineer if your application is outside of the typical funding range

Typical Project Elements

- Site prep
- Traffic control
- Roadway
- Drainage
- Illumination
- Landscaping
- Non-motorized components



Typical project elements include:

- Site preparation
- Traffic control
- Road base and surfacing improvements
- Drainage improvements
- Illumination
 - Use low energy lighting
- Landscaping
 - Limited to five percent of the eligible contract cost
- Sidewalk
 - Sidewalk is required only for reconstruction or full depth reclamation projects when
- Bicycle lanes
 - Allowed if indicated by agency's adopted bicycle plan and supported by TIB staff

What SCAP Pays For...

- Design
- Right of way
- Construction

TIB can participate in the cost of the following:

Design

- Design engineering
 - Engineering required to develop project plans, specifications and cost estimate
 - Environmental permitting
 - Cultural resource assessment
- Right of way
 - Acquisition of right of way required to construct the project
 - Legal and administrative fees associated with right of way acquisition

Construction

- Construction engineering
 - Contract administration, construction inspection, surveying and material testing
- Construction Other-
 - Work completed by local forces, utilities and/or railroad outside of the primary contract
 - Agency purchase of signals, illumination or other approved components outside of the primary contract
- Construction contract
 - Contract to complete approved scope of work

What SCAP Does Not Pay For...

- Costs exceeding WAC limitations
- Excess property
- Work outside of limits or scope
- New utilities or utility upgrades



- Engineering costs exceeding the limitations set in WAC 479-05-170
 - Design and construction engineering cannot exceed 30 percent of the eligible construction contract plus construction other cost
 - Construction ready projects are limited to 20 percent of the eligible construction contract plus construction other cost
 - Surveying and materials testing costs, even if part of the contract costs, are considered construction engineering
- Landscaping costs above the limitation set in WAC 479-05-130
 - Limited to five percent of the total eligible construction contract amount
- Right of way in excess of what is needed to construct the project
- Work outside of the project limits or approved scope
- New utilities or utility upgrades

SCAP Rating Criteria

Criteria	Points
Safety	40
Pavement Condition	30
Local Support	20
Sustainability	10
Total Points	100

Safety

40 point max

- Crash data
- Safety hazards
 - Hazards
 - Existing conditions

Collision data (15 point maximum)

- Property damage only
- Injury
- Fatality

Point Range

1 point per crash
3 points per injury
10 points per fatality

Request crash data from WSDOT as soon as possible

Potential safety hazards (20 point maximum)

- The project must eliminate or minimize the hazard to receive points
- Points will be assigned in the following categories based on severity of the hazard:
 - sight distance, existing drainage, skewed intersection, posted school zone, existing sidewalk condition, railroad crossing, control access and/or parking, obstructions, existing lighting, turn pockets, and adjacent terrain

Existing conditions (20 point maximum)

- | | |
|---------------------------------|------|
| • Pavement width | 0-15 |
| • Truck route | |
| - T-1 to T-3 | 5 |
| - T-4 | 4 |
| - T-5 | 2 |
| - Signed local truck route | 1 |
| • Bus route (transit or school) | 0-3 |

Pavement Condition 30 point max

- Existing pavement condition
 - Reconstruction or pavement rehabilitation
 - Overlay
- New street



Existing pavement condition (30 point maximum)	Point Range
<ul style="list-style-type: none">• TIB evaluates the cost effectiveness of the proposed improvement<ul style="list-style-type: none">- Rehabilitation is preferred to full reconstruction	
<ul style="list-style-type: none">• TIB staff determine the pavement condition rating (PCR)<ul style="list-style-type: none">- Reconstruction or pavement rehabilitation projects<ul style="list-style-type: none">▪ Projects with PCR between 0 and 65 receive points▪ Optimal range is 35 and below- Overlay projects<ul style="list-style-type: none">▪ Projects with PCR scores between 30 and 65 receive points▪ Optimal range is 40-50	0-30
<ul style="list-style-type: none">• New street<ul style="list-style-type: none">- Points based on significance of the road	10-15

Local Support

20 point max

- Local match
- Network development
- Connectivity
 - Direct access
 - Indirect access

Local match (5 point maximum)

- 1 point for every one percent above minimum match
 - Non-eligible cost **is not** considered as local match

Point Range

0-5

Network development (5 point maximum)

- Extends improvements
- Completes gap
- New route

0-3

0-5

0-5

Connectivity (20 point maximum)

- Direct access - facility is within the project limits
- Indirect access - facility is within 2-3 blocks of the project limits on the same corridor
 - Central business district
 - Commercial development (2 points per facility)
 - Industrial area
 - Schools (2 points per facility)
 - Public facilities (2 points per facility)
 - Medical facilities
 - Senior housing

0-5

0-5

0-3

0-6

0-6

0-5

0-2

Sustainability

10 point max



Sustainability (10 point maximum)

Point Range

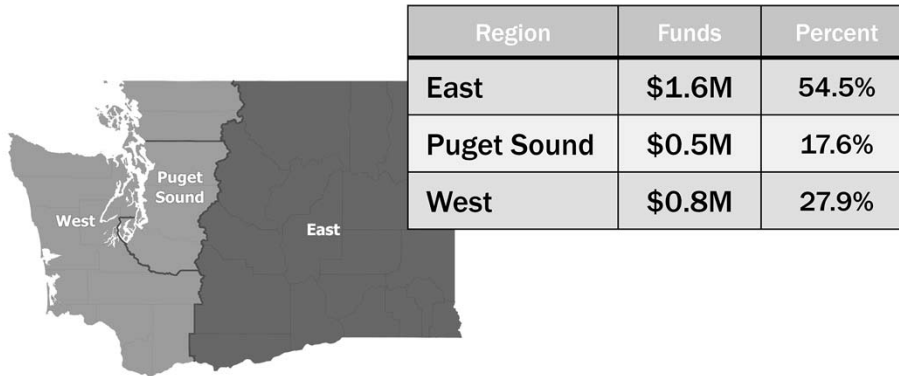
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|--|-----|
| • Adopted greenhouse gas emissions policy | 0-1 |
| • Sidewalk/buffer strip | 0-3 |
| - Sidewalk width greater than 5-foot TIB minimum | |
| - Buffer strip (3 feet or greater) | |
| • Sidewalk network development | 0-2 |
| • Hardscaping or climate-appropriate plantings | 0-1 |
| - No permanent irrigation | |
| • Low energy street lights | 0-3 |
| • Recycled material usage | 0-1 |
| • Low impact drainage practices | 0-2 |

Small City Sidewalk Program (SCSP)

- Improve pedestrian safety and enhance pedestrian mobility by providing access, system continuity and connectivity
- Projects provide facilities for transportation not recreation

SCSP Regional Distribution

\$3.0 Million Statewide



SCSP funding is distributed to three regions based on the small city populations updated annually by the Office of Financial Management

Sidewalk Program Requirements

- Eligibility
- Local match
- Sidewalk
- Typical grant size
- Project cost estimate
- WSDOT concurrence

Eligibility

- Incorporated cities and towns with populations less than 5,000 are eligible
 - Street must be:
 - City owned
 - Paved and connected to other paved streets
 - Does not include alley ways
 - Does not include dead end streets unless they provide access to significant pedestrian generators

Minimum local match

- Based on Office of Financial Management April 1 population count
 - Population
 - Under 1,000
 - 1,000 to 4,999
- | | |
|--|---------------|
| | Minimum Match |
| | 0 percent |
| | 5 percent |

Sidewalk

- Required on one side of street and ADA compliant
- Minimum width five feet clear with hard, smooth surface (concrete, asphalt)
- Separated from travel lanes with curb or physical barrier

Typical grant size

- Typical funding ranges from \$150,000 to \$300,000 per project
- Project has logical limits
- Contact your TIB Engineer if the application is outside of the typical funding range

Project cost estimate

- Ensure cost estimate includes all components of work
- Contingency reflects project complexity and uncertainty
- Estimate reviewed and signed by a professional engineer registered in the state of Washington

WSDOT concurrence

- Written WSDOT concurrence required for projects that are on or connecting to a state highway right of way

What Sidewalk Program Pays For...

- Design
- Construction



The following elements should be included in the estimate of project cost

Design

- Design engineering
 - Engineering required to develop project plans, specifications and cost estimate
 - Environmental permitting
 - Cultural resource assessment

Construction

- Construction engineering
 - Contract administration, construction inspection, surveying and material testing
- Construction Other-
 - Work completed by local forces, utilities and/or railroad outside of the primary contract
 - Agency purchase of signals, illumination or other approved components outside of the primary contract
- Construction contract
 - Contract to complete approved scope of work

What Sidewalk Program Does Not Pay For...

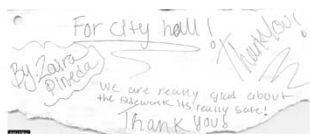
- Work outside approved scope or limits
- Right of way
- Non-eligible improvements
 - Parking
 - Street widening
 - Utilities



- Work outside of the approved scope or project limits is not eligible
- Costs exceeding WAC limitations
 - WAC 479-05-170 limits design and construction engineering to a maximum of 30 percent of the eligible construction contract plus construction other
 - Construction ready projects are limited to 20 percent of the eligible construction contract plus construction other
- WAC 479-05-130 limits landscaping cost to five percent of the total eligible construction contract amount
- Right of way acquisition is not eligible under the Small City Sidewalk Program
 - Right of way acquisition cost cannot be used as local match
- Sidewalk Program funding cannot be used for the following improvements:
 - Parking
 - Street widening
 - Installation of utilities or utility upgrades (e.g. water, sewer, power)

Sidewalk Project Attributes

- Project types
- Typical scope



Project types

Focus on completing sidewalk system on a corridor

- Construction of new sidewalk
- Completing gaps in an existing sidewalk system
- Reconstruction of existing sidewalk
- Sidewalk maintenance – panel and curb replacement
- Project may be combination of all types

Typical scope

- Site preparation
- Sidewalk
- ADA ramps
- Stormwater improvements required because of sidewalk construction
- Illumination
- Pedestrian signals
- Landscaping
 - Limited to five percent of eligible contract

Sidewalk Program Rating Criteria

SCSP Criteria	Points
Pedestrian Safety	55
Pedestrian Connectivity	30
Sustainability	10
Local Support	5
Total Points	100

Pedestrian Safety

55 point max

- Existing conditions
- Crash history
 - Request crash data from WSDOT
- Existing hazards



Existing condition (30 point maximum)

- Posted speed
- Pedestrian walking route
 - o In travel lane
 - o On shoulder
 - o On sidewalk

Point Range

0-10

20

0-18

0-18

Crash history (25 point maximum)

- Correctable Pedestrian/Vehicle crash
 - 10 points per correctable crash
- Correctable Pedestrian Only incident
 - 5 points per incident

0-20

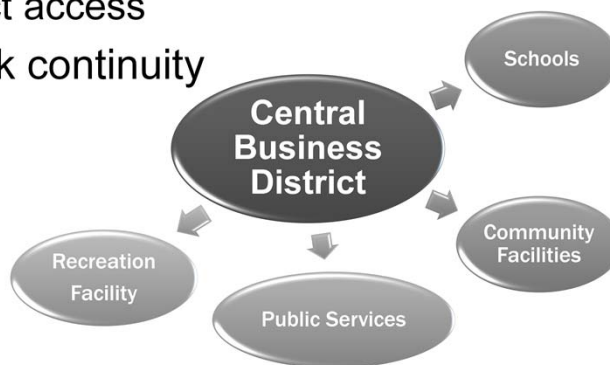
0-15

Existing hazards (15 point maximum)

- The project must eliminate or minimize the hazard to receive points
- Points are assigned for the following categories based on severity of the hazard:
 - Sight distance
 - Deep ditches
 - Truck volume
 - Traffic volume
 - Obstructions
 - Existing lighting
 - Drainage/snow issues
 - Posted school zone

Pedestrian Connectivity 30 point max

- Facilities served
 - Direct access
 - Indirect access
- Sidewalk continuity



Direct access (30 point maximum)

- To be considered direct access, the facility must be within the project limits

Indirect access (15 point maximum)

- The facility is outside the project limits but within 2-3 blocks of the project limits on the same corridor and is connected to the project by an ADA-accessible facility

	Point Range
- Schools (3 points per facility)	0-9
- Public facilities (2 points per facility)	0-6
- Recreational facilities	0-5
- Central business district	5
- Commercial area (2 points per facility)	0-5
- Industrial area	0-3
- Medical facilities	0-3
- Senior housing	0-2
- High density housing	0-2
- Signed transit stop	0-2

Sidewalk continuity (5 point maximum)

- Completes gap	5
- Extends existing sidewalk	3

Sustainability

10 point max



Sustainability (10 point maximum)

Point Range

- Adopted greenhouse gas emissions policy 0-1
- Sidewalk or buffer strip 0-3
 - Sidewalk width wider than 5-foot TIB minimum
 - Buffer strip (3 feet or greater)
- Sidewalk network development 0-2
- Hardscaping or climate-appropriate plantings 0-1
 - No permanent irrigation
- Low energy street lights 0-3
- Recycled material usage 0-1
- Low impact drainage practices 0-2

Local Support

5 point max

- Local Match above minimum required
 - 1 point for each 1 percent above minimum match



- Non-eligible costs are not considered as local match

Small City Preservation Program (SCPP)

Intent

- The legislature recognized that the small cities are unable to fully maintain and preserve their street infrastructure system
- Preserve and improve the condition of small city streets

What does it fund?

- Chip seal, overlay and full depth reclamation (FDR) of existing paved streets in incorporated cities with populations less than 5,000

Anticipated 2017 call size is \$6M

SCPP Requirements

- Eligibility
- Requirements



Eligible agencies

- Incorporated Cities with a population under 5,000

Eligible streets

- Hard surfaced public streets within the city limits

Non-eligible streets

- Gravel streets
- State highways

Minimum local match

- Based on city assessed valuation updated annually by the Department of Revenue
- Assessed Valuation

	Minimum Match
- Under \$100 million	0 percent
- \$100 million to \$500 million	5 percent
- Over \$500 million	10 percent
- ADA Ramps
 - Overlay and full depth reclamation projects must upgrade existing ADA ramps to current standards
- WSDOT concurrence
 - Written WSDOT concurrence required for projects that intersect a state highway

SCPP Project Development

- Design Engineering
- Construction Engineering
- Construction Contract

Design Engineering

- Design required to develop project plans, specifications and cost estimate

Construction Engineering

- Construction inspection and contract administration

Construction Contract

- Contract to complete approved scope of work

SCPP Project Type

- Full Depth Reclamation
- Overlay
- Chip Seal



Full Depth Reclamation (FDR)

- Suitable treatment if road base is failing
- Pulverize existing surface into base
 - May include base amendment
- Overlay surface
- Striping
- Minor drainage improvements
- Requires ADA ramp upgrade to current standards

Overlay

- Overlay of existing surface
- Pavement repair and crack sealing
- Striping
- Minor drainage improvements
- Requires ADA ramp upgrade to current standards

Chip Seal

- Chip seal of existing surface
- Pavement repair and crack sealing
- Sweeping
- Striping
- Minor drainage improvements

SCPP Full Depth Reclamation Rating

Criteria	Points
Segment Rating	80
Agency Rating	20
Total Points	100

Full Depth Reclamation (FDR)

Segment rating (80 point maximum)	Point Range
<ul style="list-style-type: none"> • Pavement condition rating (PCR) <ul style="list-style-type: none"> - PCR less than 60 - 25 percent and above medium and high severity alligator cracking - Higher points for lower PCR and higher percent alligator cracking • Type of route <ul style="list-style-type: none"> - Major street - Minor street • Sidewalk maintenance 	<p>0-60</p> <p>10</p> <p>5</p> <p>0-10</p>
<p>Agency rating (20 point maximum)</p> <ul style="list-style-type: none"> • Economy of scale <ul style="list-style-type: none"> - Documented response from provider required with application • Prior SCPP funding • Agency PCR average 	<p>10</p> <p>0-5</p> <p>0-5</p>

SCPP Overlay Rating

Criteria	Points
Segment Rating	80
Agency Rating	20
Total Points	100

Overlay

Segment rating (80 point maximum)

- Pavement condition rating (PCR)
 - PCR between 30 and 65
 - Less than 25 percent medium and high severity alligator cracking
 - Higher points for lower PCR and lower percent alligator cracking

Point Range

0-60

- Type of route

- Major street
- Minor street

10

5

- Sidewalk maintenance

0-10

Agency rating (20 point maximum)

- Economy of scale
 - Documented response from provider required with application
- Prior SCPP funding
- Agency PCR average

10

0-5

0-5

SCPP Chip Seal Rating

Criteria	Points
Segment Rating	70
Agency Rating	30
Total Points	100

Chip Seal

Segment rating (70 point maximum)

Point Range

- Pavement Condition Rating
 - PCR range is 40 to 90
 - Less than 25 percent medium and high severity alligator cracking
 - Higher points for midrange PCR and lower percent alligator cracking

0-70

Agency rating (30 point maximum)

- Economy of scale
 - Documented response from provider required with application
- Segment continuity
- Prior SCPP funding
- Agency PCR average

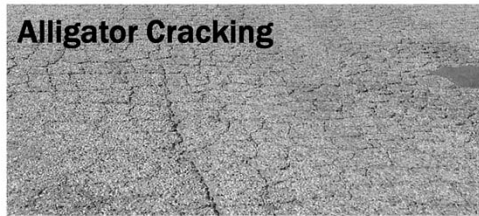
10

0-10

0-5

0-5

Pavement Distresses



TIB engineers use the distresses shown above to determine the pavement condition rating

SCPP Application Suggestions

- FULL DEPTH RECLAMATION (FDR)
 - Typical grant amount less than \$350K
 - Logical project limits
 - Pavement Condition Rating less than 60
 - 25 percent or more alligator cracking
 - Include striping in estimate
 - ADA ramps must be upgraded

- Break street into segments **only** if geometrics change
- Estimate needs to break down cost by segment
 - TIB may not select all segments

SCPP Application Suggestions

- OVERLAY
 - Typical grant amount \$75K-\$200K
 - Logical project limits
 - Pavement condition rating 30 to 65
 - Less than 25 percent alligator cracking
 - Include prep work and striping in estimate
 - ADA ramps must be upgraded

- Break street into segments **only** if geometrics change
- Estimate needs to break down cost by segment
 - TIB may not select all segments

SCPP Application Suggestions

- CHIP SEAL
 - Typical grant amount \$25K-\$100K
 - Pavement condition rating 50 to 80
 - Less than 25 percent alligator cracking
 - Include prep work, sweeping and striping in estimate
 - Sidewalk maintenance not eligible
 - ADA ramps not required

- Break street into segments **only** if geometrics change
- Estimate needs to break down cost by segment
 - TIB may not select all segments

Other Funding Opportunities

- Small City Federal Match Program
- Relight Washington
- Complete Streets



Small City Federal Match Program

- TIB provides the required local match for federal transportation funding
 - Up to 10% of the Small City Arterial Program in TIB funding available annually
 - First-come, first-serve basis until funds are exhausted
 - Non-competitive program
- Project must meet eligibility requirements for Small City Arterial Program (SCAP) or Small City Sidewalk Program (SCSP)

Relight Washington

- Working with all energy providers if there is a savings
- PUDs are approved - except Lewis, Cowlitz, and Grant
- If your agency still needs converted and can prove savings, contact your Region Engineer

Complete Streets

- Nominations anticipated in 2018 with awards in the first half of 2019
- No need to wait for open nominations, it is a good practice to involve nominators in all projects now
 - State Agencies: Department of Archeology & Historic Preservation, Department of Commerce, Department of Health, Department of Transportation
 - Non-Governmental Organizations: Community Transportation Association – NW, Feet First, Futurewise, Transportation Choices Coalition, WA Bikes/Cascade Bicycle Club
- See TIB website for Complete Streets Program information

Project Selection

- Board selection on **November 17, 2017**
- Results posted on the TIB website www.tib.wa.gov
- TIB notifies all agencies of results

Project Administration

- Submit within one year of project selection
 - Project Funding Status Form
 - Fuel Tax Agreements
 - Adopted TIP
- Engineering
- Project issues
- Project billings

Within one year of project selection submit the following

- Signed project funding status form confirming that the funding partners are fully committed in accordance with RCW 47.26.084
- Signed fuel tax agreements
- Adopted six-year transportation improvement program (TIP)
 - Selected SCAP and SCSP projects must be included in the local agency Six Year TIP prior to receiving TIB authorization to proceed with project work
 - You are **not required** to include SCPP projects on your adopted TIP

Engineering

- Simple projects are expected to come in **LESS** than the 30% maximum
- Design engineering cost limit recommended to be no more than 15% of engineer's estimate including contingencies
- Construction supplement or agreement will not be accepted until after bids have opened and contract cost is known. Design and Construction engineering is limited to a **maximum of 30%** construction cost plus construction other

Issues may arise during design and construction of your project

- Contact your TIB Project Engineer to discuss any issues that affect schedule, budget or scope:
 - Schedule delays
 - Funding shortfalls
 - Changes to approved scope/project limits
 - Change orders during construction

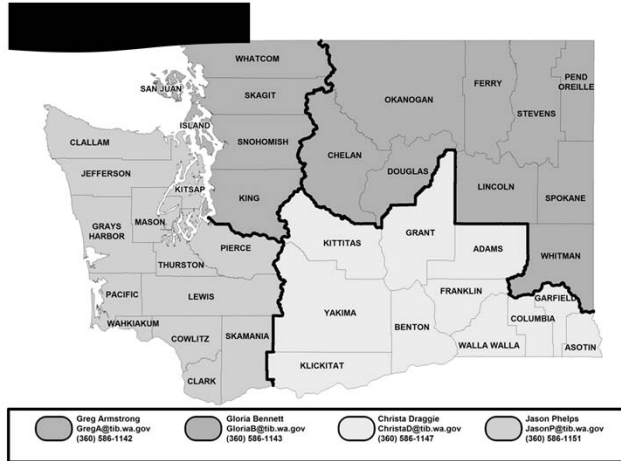
TIB expects agencies to request payment regularly during the life of the project

- Submit billing requests as costs are incurred
- Design phase - Submit payment requests to TIB at least quarterly during design phase
- Construction phase - Request payment from TIB monthly during construction of the project

Feedback

- Looking for YOUR feedback on:
 - SCAP
 - SCSP
 - SCPP
 - Other Programs
 - Complete Streets
 - Emergency Repair
 - Federal Match
 - Relight Washington

TIB Project Engineers



Your TIB Engineer contact is shown on all TIB application forms
Use these contacts for questions during the 2017 application cycle

Questions